

INDUSTRIAL VEHICLE CROSSINGS

Dear Sir/Madam,

Council's requirements for the construction of industrial vehicle crossings of the gutter and footway (area between kerb and property boundary) are set out below. To ensure crossings are properly constructed, additional work may be required such as:

- re-alignment or extension of gutter crossing
- reconstruct damaged gutter crossing
- connect roof water pipeline to kerb

An authorised Council officer will advise of any additional work required at the time of formwork inspection. If you have any questions about any additional work or any construction that may be required, please ring 6538 5250 between 8.30 am and 4.30 p.m.

TYPES OF CONSTRUCTION PERMITTED

Footway crossings must be constructed in plain concrete. Crossings will be restored by Council if at some future date they are disturbed by the activities of Public Utility Authorities or Council.

The finished surface must be sufficiently rough so as to ensure safety for pedestrians and other users. The concrete surface must NOT be finished in a smooth steel float finish.

All construction must be in accordance with the attached Standard Industrial and Commercial Footway Crossing plan and Specification for Industrial Vehicle Crossings in Concrete.

COUNCIL FEE

See Annexure 'A' for Council Fees for a vehicular crossing.

PREMATURE & ADDITIONAL INSPECTIONS OF VEHICULAR CROSSING CONSTRUCTION

See Annexure 'A' for Council Fees for a vehicular crossing.

HOW TO ARRANGE CONSTRUCTION

Council does not construct vehicle crossings, and therefore a licensed concreter must be engaged. Gutter crossings (laybacks), extensions to gutter crossings or the reinstatement of gutter crossings to kerb and gutter MUST be constructed by a licensed concreter.

Names of concreters can be found in the classified section of local papers or under "Concrete Contractors" and "Paving - Concrete" in the "yellow pages".

Concreters must provide a \$1000 bond before they are permitted to undertake construction works.

Works left incomplete by concreters will be finished by Council at their cost. Money will be deducted from the bond and no further applications will be accepted.

INSPECTIONS (FIRST AND FINAL)

The work must be inspected by Council when the formwork is erected (in the case of concrete work) or the base prepared (in the case of a rural type crossing). The work must again be inspected on completion. Please ensure that the formwork has been removed and the area is backfilled before booking in the final inspection. Ring 6538 5250, giving 48 hours minimum notice for the inspection bearing in mind that applications must be received at least two working days prior to inspections being requested.

WORKMANSHIP

Council may require reconstruction of the work if the workmanship or the finish is not satisfactory or if the work is not in accordance with Council plans and specifications.

FUTURE CRACKING, SETTLEMENT, ETC.

Council cannot guarantee that cracking, settlement, etc., of the construction will not occur in the future and Council will not be liable for any claims to repair or reconstruct crossings even though Council had originally inspected and passed the work.

NOTE 1.

Council accepts NO responsibility for the identification or position of property boundaries (either at the street or along side boundaries).

NOTE 2.

The concreter is to comply with Occupational Health and Safety and WorkCover requirements and all other requirements associated with this construction.

NOTE 3.

It is the responsibility of the concreter carrying out the driveway construction to ensure the site is kept safe and pedestrians have safe access around the construction site. A traffic control plan must be produced upon request by an authorised Council officer. The traffic control plan must comply with AS 1742.3.

NOTE 4.

Soil erosion and sedimentation control measures are to be maintained during the entire construction period until distributed areas are restored. Infringement Notices including a monetary penalty may be issued by Council where the maintenance measures fail to meet minimum standards required by Council's policy and Acts of Law.

Yours faithfully,

Danny Green
GENERAL MANAGER

Per:

CONSTRUCTION SPECIFICATION

INDUSTRIAL VEHICLE CROSSINGS IN CONCRETE

1. All gutter crossings (laybacks) shall be placed on a compacted layer of approved fine crushed rock, 175mm thick or extending down to the base of the existing pavement, whichever is the greater depth.
2. Laybacks shall be formed integrally with the gutter section except in the case of existing roll kerb. Refer to attached Standard Industrial and Commercial Footway Crossing plan.
3. Where a new gutter crossing (layback), or an extension to an existing gutter crossing and footway crossing is to be constructed where there is existing kerb and gutter, the gutter as well as the kerb is to be saw cut at each end and the gutter completely removed as well as the kerb. No mastic joint will be allowed in laybacks and mastic joints in kerb and gutter to be a minimum of 1m apart.
4. Before pouring the new gutter crossing it may be necessary to tack an edge board to the surface of the road adjacent to the lip of gutter so that the correct edging tool can be used. If there has been damage to the edge of the road pavement then the concrete is to be allowed to spill underneath the edge board and then, a suitable time after edging the lip of gutter, this edge board is to be removed and the top 25mm of the concrete protruding into the road pavement is to be trowelled out. The edge of the road pavement is then to be restored with hot mix or cold mix.
5. Concrete crossings are to consist of 150mm thick concrete on 25mm sub-base of approved granular material. Any existing 75mm concrete path paving is to be removed and reconstructed in accordance with this specification. All unsuitable material under the proposed apron must be removed and replaced with approved compacted material to a suitable depth.
6. Concrete shall be reinforced with F82 (SL82) fabric on chairs and bases.
7. Concrete shall have a minimum 28 day strength (F'c) of 20MPa and must be kept constantly moist for 5 days after pouring to allow the concrete to cure.
8. Concrete shall have a light brushed finish on vehicle crossings and a steel floated finish on laybacks and on kerb and gutter.
9. All vehicle crossings must be constructed with a crossfall of 4% from top of kerb to the boundary. Any variation to the standard crossfall must have prior approval of Council.
10. Joint at street alignment to be Mastic and dowel joint formed using bitumen impregnated fibre board for the full depth of the concrete with 20dia. galvanised steel dowels. Refer Note 9 of the Standard Industrial and Commercial Footway Crossing plan attached.
11. Before the formwork inspection for a concrete vehicle crossing can be made, the formwork must be adequately fixed in a place to the correct levels; there must be a 25mm layer of approved fine granular material in the bottom of the excavation and all joints must either be in place or on site where the Council officer can see them.
12. All construction is to be in accordance with Council's Standard Industrial and Commercial Footway Crossing Plan attached.

13. Minimum driveway width at boundary to be 4 metres except where otherwise directed by developers consent conditions.
14. The concreter carrying out work must be licensed and must have in force public liability insurance and at all times maintain the safety of the site to WorkCover requirements.
15. The concreter must have on site, a traffic control plan that complies with the Australian Standard 1742.3 and/or "The RTA Traffic Control At Work Sites" Manual and must be produced to the Council officer upon request.

ANNEXURE 'A'

COUNCIL FEE

An application fee in accordance with Councils current fees and charges must be paid a minimum of four working days prior to the commencement of any works to cover supervision and administrative costs. Urgent applications can be processed sooner however a surcharge fee may apply in addition to the standard application fee. When paying the fee, the name, address, telephone number and licence number of the concreter engaged must be given in writing. Please fill out the attached application and return it to Council with your payment. Ensure that the concreter chosen has the \$10,000,000 Public Liability Insurance cover and has lodged a bond with Council and a copy of the Policy.

BOND

Concreters/contractors are required to pay a crossing bond. The crossing inspection fee will need to be paid separately with each application. In case of incomplete or unsatisfactory works, Council may undertake necessary works to comply with Council's specification at the cost of the concreter. No further applications will be accepted until the crossing bond is topped up.

PREMATURE & ADDITIONAL INSPECTIONS OF VEHICULAR CROSSING CONSTRUCTION

Officers are at times requested to carry out inspections of sites in response to claims by applicants that works are complete or ready for inspection when in fact they are not. Where officers are required to re-inspect as a result of such a premature request for an inspection, a fee will be charged. No re-inspection will be carried out until this fee is paid.

It should be noted that the administration fee covers the first formwork and final inspection only. All extra inspections required will attract a fee.

Cancellations must be made prior to 9:30am by phoning 6538 5250. Failure to cancel will incur an additional fee.

All fees are in accordance with Council's adopted fees and charges. For further information, please see Council's Web Page or contact Council's Office during office hours.

Notice Regarding Telstra Pits and Other Service Pits

Please be advised that Telstra have notified Council that Telstra manholes and/or pits will not be accepted in new vehicle footway crossings.

You are therefore advised to take into account Telstra manholes and pit locations prior to any construction of a dwelling to ensure that the proposed vehicle crossing will not be affected by Telstra pits.

Telstra advises that any vehicle crossing proposal that may incorporate a Telstra manhole and/or pit would be unacceptable in their view due to posing a threat to the occupational health and safety of Telstra staff and furthermore due to the adverse impact on Telstra's ability to adequately service the network and their customers.

The following procedure should be followed to obtain Telstra approval to construct a vehicular crossing in case that the Telstra manholes and pits are located in the vehicular footway crossings:-

DialBeforeYouDig at the following web address:- www.1100.com.au
DialBeforeYouDig Phone No. is 1100 Fax No: is 1300 652 077

Contact Telstra - Phone: 02 9204 0989. Their office address is:- Level 3, No 9, Wentworth Street, Parramatta 2150.

Telstra's fax number is 02 9204 9011 and the postal address is:- Locked Bag 5390, Parramatta.

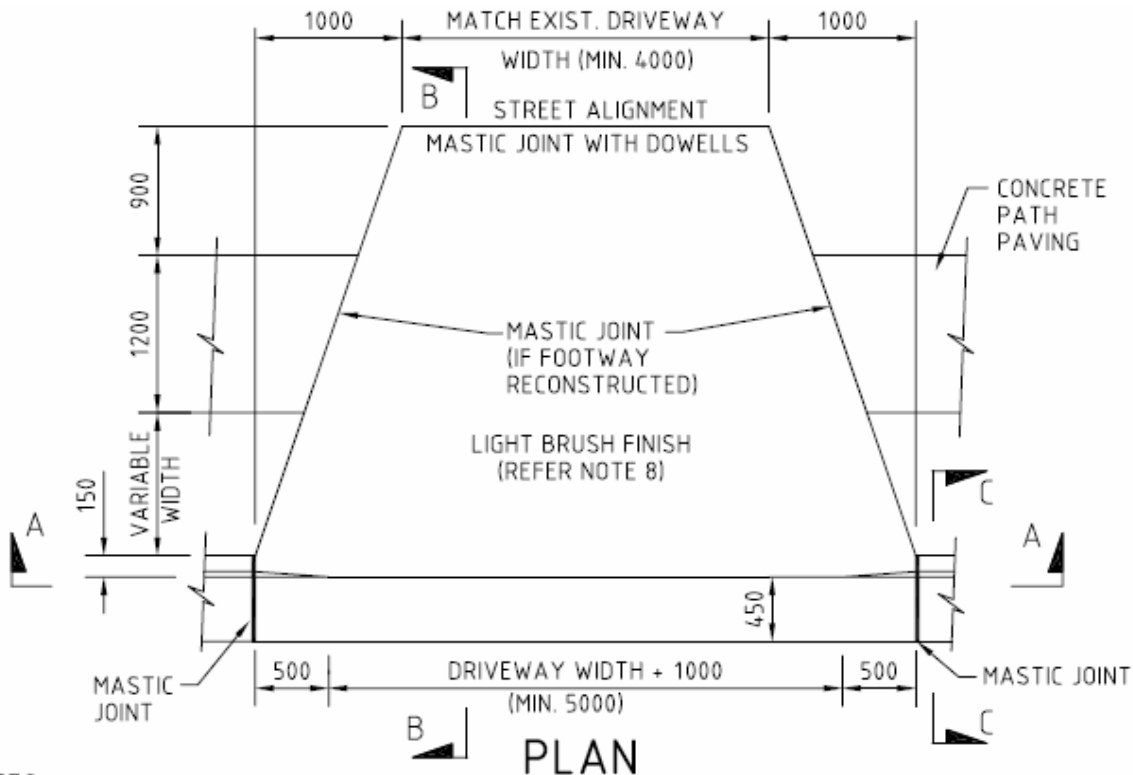
Thereafter Telstra will provide a written consent to construct vehicular crossing.

Council will not accept vehicle crossing applications that incorporate or impact on Telstra manholes and or/pits without written permission i.e. clearance by Telstra.

In addition proposed vehicle crossings must be located a minimum of 1 metre from stormwater drainage pits (residential crossings), Country Energy/Telstra poles, electrical boxes, or 2 metres from stormwater drainage pits (industrial crossings) and no closer than 6 metres to the kerb tangent point near street corners.

If you have any enquiries regarding the above please contact Council's Technical Services Section.

Standard Industrial and Commercial Footway Crossing



NOTES:

1. COMMERCIAL AND INDUSTRIAL VEHICULAR FOOTWAY CROSSINGS SHALL BE 150mm THICK CONCRETE, REINFORCED WITH RF82 MESH ON CHAIRS, 35mm COVER.
2. THE CONCRETE SHALL BE PLACED ON A 25mm LAYER OF APPROVED FINE, GRANULAR MATERIAL, EXCEPT AT THE KERB AND GUTTER LINE WHERE THIS SECTION SHALL BE PLACED ON A MINIMUM 175mm COMPACTED LAYER OF D.G.B. 20 OR THE DEPTH OF THE EXISTING PAVEMENT WHICHEVER IS GREATER.
3. MINIMUM WIDTH OF ENTRY IN INDUSTRIAL AREA AT KERB LINE SHALL BE 5m PLUS WINGS.
4. MAXIMUM WIDTH OF ENTRY IN INDUSTRIAL AREA AT KERB LINE IS SUBJECT TO TRAFFIC REGULATIONS, POLICY AND STANDARDS FOR TRAFFIC GENERATING DEVELOPMENTS.
5. EXISTING CONCRETE FOOTPATH SHALL BE SAW CUT AT APPROX. 1m EITHER SIDE OF THE CROSSING AND RECONSTRUCTED IN CONJUNCTION WITH THE CROSSING.
6. CONCRETE SHALL HAVE A 28 DAY STRENGTH (F'c) OF 20MPa AND A SLUMP OF 80mm.
7. ANY VARIATIONS TO STANDARD CROSSFALL 4% ON FOOTWAY SHALL HAVE THE PRIOR APPROVAL OF COUNCIL.
8. CONCRETE SHALL HAVE A LIGHT BRUSHED FINISH.
9. FOOTWAY CROSSING TO BE DOWELLED TO INTERNAL DRIVEWAY AND TO ROLL KERB USING GALVANISED 20% C DOWELS PLACED 500mm IN FROM EDGES OF CROSSING AND AT 900mm SPACING WITH SUITABLE MATERIAL TO ENSURE SLIP JOINT.
10. ALL DOWELS TO BE 300mm LONG WITH 150mm PENETRATION INTO EACH SLAB.
11. STANDARD LAYBACK TO BE DEMOLISHED AND RE CONSTRUCTED AS AN INTEGRAL PART OF THE PROPOSED CROSSING. IN CASE OF ROLL TOP KERB, IT IS AN OPTION TO RETAIN IT OR RE CONSTRUCT IT AS AN INTEGRAL PART OF THE CROSSING.
12. ALL DIMENSIONS ARE SHOWN IN MILLIMETRES.
13. THE SECTION OF THE CROSSING ADJACENT TO CONCRETE FOOTPATH IS TO HAVE 2.5% FALL UNLESS MATCHING THE CROSSFALL OF EXISTING FOOTPATH.